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**17/00263/FUL**  
**Land to the North of Hanworth Road and West of Costco**  
**Sunbury-on-Thames**

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## Planning Committee

28 June 2017



<b>Application Nos.</b>	17/00263/FUL		
<b>Site Address</b>	Land to the north of Hanworth Road and to the west of Costco, Sunbury		
<b>Proposal</b>	Erection of a new detached building to provide a car dealership comprising a showroom for car display, wash/valet facilities, workshop and MOT testing, with the provision of car parking, associated infrastructure and landscaping.		
<b>Applicant</b>	Diageo Pension Trust (Property Custodian) Limited and Guy Salmon Limited		
<b>Ward</b>	Sunbury Common		
<b>Call in details</b>	N/A		
<b>Case Officer</b>	Paul Tomson		
<b>Application Dates</b>	Valid: 17/02/2017	Expiry: 19/05/2017	Target: Extension of time agreed
<b>Executive Summary</b>	<p>This application involves the erection of a new building comprising 16,062 sqm gross internal floorspace to provide a car dealership (Jaguar Land Rover) comprising a showroom for car display, wash/valet facilities, workshop and MOT testing, with the provision of car parking, associated infrastructure and landscaping (Sui Generis use).</p> <p>The site is located within a designated Employment Area and the principle of erecting a commercial building of this nature is considered acceptable. Both Highways England and the County Highway Authority are satisfied that the proposed development will have a satisfactory impact on the adjacent highways. The Lead Local Flood Authority is satisfied with the drainage elements subject to the imposition of conditions. It is also considered that the relationship with the nearby residential properties in Woodberry Close, Ashridge Way and Windsor Road will be acceptable. The proposed design and appearance will be in keeping with the character of the area.</p>		
<b>Recommended Decisions</b>	This planning application is recommended for approval subject to a Section 106 agreement and conditions.		

## MAIN REPORT

### 1. **Development Plan**

1.1 The following policies in the Council's Core Strategy and Policies DPD 2009 are considered relevant to this proposal:

- LO1 (Flooding)
- EM1 (Employment Development)
- EN1 (Design of New Development)
- EN3 (Air Quality)
- EN11 (Development and Noise)
- EN15 (Development on Land Affected by Contamination)
- CC1 (Renewable Energy, Energy Conservation and Sustainable Construction)
- CC2 (Sustainable Travel)
- CC3 (Parking Provision)

### 2. **Relevant Planning History**

15/00676/FUL	Erection of a new building to provide a B1c, B2 and B8 development with a total floor space of 4,338 sqm.	Approved 18/01/2016
13/01029/FUL	Demolition of existing buildings on site and the erection of a "warehouse club" (13,006sqm), including tyre installation, sales and associated facilities with access and associated parking and landscaping	Approved 15/03/2014

*(Officer Note – relates to the adjacent Costco site)*

### 3. **Description of Current Proposal**

3.1 The application relates to a site of 0.88 hectares located to the Hanworth Road (A316) in Sunbury. The site forms part of the existing industrial estate that also comprises Costco, 'Lok 'n Store' and other commercial buildings. The site is accessed off a service road which also provides access to the Costco site. The site previously formed part of the Dairy Crest building, although this was demolished prior to the commencement of the adjacent Costco development and is currently free of any buildings. To the rear is an area of open land owned by Thames Water that includes the Staines Reservoirs Aquaduct. Further to the north and west are the houses in Woodberry Close, Windsor Road and Ashridge Way.

3.2 The site is located within the urban area and is part of a designated Employment Area. The A316 is a Trunk road.

- 3.3 The proposal involves the erection of a new building comprising 16,062 sqm gross internal floorspace to provide a car dealership (Jaguar Landrover) comprising a showroom for car display, wash/valet facilities, workshop and MOT testing, with the provision of car parking, associated infrastructure and landscaping (Sui Generis use). The proposed building will measure 47.1 metres in width, 103.9 metres in depth and 15.1 metres in height. It will be faced with a mix of flat metallic grey cladding and large areas of glazing, particularly at the front of the site. The rear of the building (i.e. the area facing towards the residential properties) will be faced in a light grey/silver 'trapezoidal' cladding. 458 car parking spaces will be provided, most of which will be located within the building or on the roof level. Most of them will be accessed via a multi-level ramp located at the rear of the building, which will be entirely enclosed by metal cladding. However, 26 no. customer parking spaces will be provided, which will be located outside to the front and part side of the building. The proposed hours of opening for all elements of the premises will be from 06.00 – 23.00 hours every day. The proposed workshop element with MOT testing will be located in the front part of the building at 2<sup>nd</sup> floor level.
- 3.4 The applicant states that the proposed development will employ 141 full-time employees. The applicant also states that the Jaguar Land Rover dealership at Sunbury will be considered a Statement Site for the brand. There are to be seven Statement Sites around the world, and Sunbury will be the UK's.
- 3.5 Copies of the proposed site layout and elevations are provided as an Appendix.

#### 4. **Consultations**

- 4.1 The following table shows those bodies consulted and their response.

Consultee	Comment
County Highway Authority	No objection subject to conditions. Requests financial contributions towards transport improvements and sustainable transport measures including the cost of monitoring a travel plan (the applicant has agreed to this).
Highways England	No objection
Environment Agency	Any response will be reported orally at the meeting. Did not wish to comment on the previous scheme (15/00676/FUL).
Thames Water	No objection subject to a condition relating to waste water drainage.
Surrey County Council Lead Local Flood Authority	No objection subject to conditions
London Borough of Hounslow	Any response will be reported orally at the meeting. Did not object to the previous



	application.
County Archaeologist	No objection
Crime Prevention Officer	No objection. Made various security related comments. A copy of the response has been forwarded to the applicant.
Environmental Health (Pollution)	No objection on contaminated land grounds subject to conditions. With regard to air quality, no objection subject conditions and a financial contribution of £6,000 towards the cost of providing a public electric vehicle charging point in the local area.
Environmental Health (Noise)	No objection subject to conditions
Sustainability Officer	No objection
Economic Development Officer	Supports the proposal and considers that it will be a significant economic asset to the Borough.

## **5. Public Consultation**

- 5.1 73 properties were notified of the planning application. A site notice was displayed and notice was provided in the local press. 4 letters of objection have been received. Reasons for objecting include: -
- The height and size of the building and the visual impact in relation to the dwellings to the north.
  - Inadequate details regarding landscaping
  - Noise and disturbance. The proposed opening hours are excessive. Concern about the noise and disturbance (including vibrations) during the construction phase.
  - Increase in the use of the A316 slip road and concern that this will adversely affect highway safety

## **6. Planning Issues**

- Principle of Development
- Design and appearance
- Impact on neighbouring residential properties
- Traffic generation and impact on adjacent highways
- Parking
- Air Quality

## **7. Planning Considerations**

### **Employment Development**

- 7.1 Policy EM1 (Employment Development) of the Core Strategy and Policies DPD (CS & P DPD) states that the Council will maintain employment development by: -

- a) Retaining designated Employment Areas and supporting in principle proposals in these areas for employment development.
  - b) Encouraging proposals for redevelopment and extensions that enable business needs to be met and make the most effective use of available employment land.
  - c) Refusing proposals that involve a net loss of employment land or floorspace in Employment Areas, unless the loss of floorspace forms part of a redevelopment that more effectively meets needs for an existing business operating from the site, or the loss of employment land is part of a mixed use development from the site which results in no net loss of employment floorspace, or it can be clearly demonstrated that the maintenance of existing levels of employment floorspace on the site is unsustainable and unviable in the long term.
- 7.2 The site is located within a designated Employment Area and the principle of redeveloping the site to provide new commercial buildings is considered acceptable. Up until relatively recently, the site was occupied by the Dairy Crest building, which also extended over to include the land now occupied by Costco. The combined floorspace of the proposed development and the Costco building is substantially more than the floorspace of the former Dairy Crest building, and consequently there will be no loss of employment floorspace. There will also be no loss of employment land. Accordingly, the proposed complies with Policy EM1. It is relevant to note that the applicant states that the proposed development will employ some 141 full time employees.
- 7.3 The Council's Corporate Plan has four key priorities and one of these is Economic Development. The proposal will assist in achieving the identified aim "to stimulate more investment, jobs and visitors to Spelthorne to further the overall economic wellbeing and prosperity of the Borough and its residents." The Council's Economic Strategy 2017 contains an action plan to grow and foster economic development. The Strategy seeks, amongst other things, to "support continuing inward investment to the Borough" and "champion the needs of businesses and, where appropriate, assist in fostering new or existing business sectors." This planning proposal will fully accord with the aims of the Economic Strategy.

#### Design and Appearance

- 7.4 Policy EN1a of the Core Strategy and Policies DPD (CS & P DPD) states that the Council will require a high standard in the design and layout of new development. Proposals should demonstrate that they will create buildings and places that are attractive with their own distinct identity; they should respect and make a positive contribution to the street scene and the character of the area in which they are situated, paying due regard to the scale, height, proportions, building lines, layout, materials and other characteristics of adjoining buildings and land.
- 7.5 The proposal is considered to be in keeping with the character of the area and complies with Policy EN1. The building will be seen in context with the other large scale commercial buildings in the surrounding area. Whilst the building

will be 15.1 metres in height, this is a similar height to the adjacent Costco building which is 14 metres. Furthermore, the proposed building line will be set back from the A316 road by a similar distance to the neighbouring Costco and Lok 'n' Store buildings and will not appear visually obtrusive in the street scene. It is also considered that the proposed elevational treatment including the mix of grey and silver coloured metal cladding is acceptable in this industrial estate context. The proposed front elevation will include substantial areas of glazing at ground and first floor levels, which will give the appearance of an active frontage and make a positive contribution to the area.

#### Impact on Neighbouring Residential Properties

- 7.6 Policy EN1b of the CS & P DPD states that new development should achieve a satisfactory relationship to adjoining properties avoiding significant harmful impact in terms of loss of privacy, daylight or sunlight, or overbearing effect due to bulk and proximity or outlook.
- 7.7 The only residential properties that are situated close to the application (but do not adjoin it) are the dwellings at the southern end of Woodberry Close, and the houses in Ashridge Way and Windsor Road. Whilst the third party representations comments regarding the height of the building are noted, it is considered that an objection could not be raised on amenity grounds. The proposed rear elevation will be at least 60 metres away from the rear elevations of the neighbouring properties which is considered acceptable. There is a 27 metre wide strip of land owned by Thames Water which acts as a buffer between the application site and the existing residential area to the north. The new building will not break a 25 degree line in relation to the neighbouring dwellings. This criteria is set out in the Supplementary Planning Document (SPD) on Residential Extensions and New Residential Development 2011 and is used to demonstrate that a new residential building will not have an overbearing impact. Whilst the proposal is a commercial building, the 25 degree rule in the SPD nevertheless provides useful guidance by showing that the proposed building will not have an overbearing impact on the nearby residential properties. There will be no windows in the proposed rear elevation and consequently no overlooking. Furthermore, I do not consider there would be any adverse overlooking from the roof level car parking area. It is relevant to note at the proposed rear elevation will be faced with a pale grey/silver cladding and that the building will be seen in a light tone (similar in tone to a cloudy sky) when viewed from the nearby dwellings.

#### Noise

- 7.8 Policy EN11 of the CS & P DPD states that the Council will seek to minimise the adverse impact of noise by requiring developments that generate unacceptable noise levels to include measures to reduce noise to an acceptable level.
- 7.9 The Council's Environmental Health Officer (EHO) was consulted and has raised no objection subject to the imposition of a condition requiring the submission of a Noise Management Plan to be approved in writing by the Local Planning Authority. This will ensure that the Council has control over the hours of deliveries and other activities so not to cause harm to the amenity of

neighbouring properties, especially during evening and weekend hours. It is recognised that the proposal includes the provision of a vehicle workshop and MOT testing station, and that the whole of the building will have operating hours Monday to Sunday 06.00 – 23.00 hours. However, it is important to note that the workshop and MOT testing station is to be located within the building at 2<sup>nd</sup> floor level. It is also located at the front half of the building (i.e. towards the Hanworth Road elevation). Whilst access to these upper levels will be via the ramp at the rear of the building, this will be entirely enclosed with metal cladding and noise from the traffic movements will be largely contained. Consequently, subject to the condition requiring the Noise Management Plan, it is considered that the proposal will not have an adverse noise impact on neighbouring properties. The Environmental Health Officer has also recommended conditions controlling noise from any plant, and controlling transport hours during the construction phase.

#### Parking and Transport Matters

7.10 Policy CC2 of the CS & P DPD states that the Council will seek to secure more sustainable travel patterns by only permitting traffic generating development where it is or can be made compatible with the transport infrastructure in the area taking into account:

- (i) number and nature of additional traffic movements, including servicing needs,
- (ii) capacity of the local transport network,
- (iii) cumulative impact including other proposed development,
- (iv) access and egress to the public highway,
- (v) highway safety.

7.11 The proposed development is accessed from the A316 Trunk road and consequently Highways England were consulted on the application. The County Highway Authority were also consulted as many of the vehicles associated with the development will be using nearby non-Trunk road highways (e.g. the A308). With regard to the A316, Highways England have raised no objection to the proposal. They are satisfied that the applicant has demonstrated that a car transporter lorry can turn around within the site and exit in forward gear, and that there will be no adverse impact on the adjacent highway. With regard to the A308 and other non-Trunk roads, the County Highway Authority (CHA) has raised no objection subject to conditions. They have also requested a financial contribution of £60,150 towards transport improvements and measures to encourage sustainable transport. The CHA consider that the proposal, when combined with the adjacent Costco development, will generate an increase in the number of vehicle trips onto the highway network compared to the former Dairy Crest development. The CHA have made the following comments on the increase in vehicle movements:

*“The County Highway Authority (CHA) does not agree with the developer's trip rates and forecast vehicle movements. The CHA is of the view the*

*proposed development is likely to generate 66 more vehicle movements during the am peak and 38 more vehicles movements during the pm peak than the extant use of the site. This is based on the sites which the CHA has selected from TRICS.*

*This increase is likely to have an impact at Sunbury Cross signal junction which is controlled by Highways England. Highways England have not asked for any contribution or amendments to the junction. However CHA roads feed into the junction, so in order to encourage use of none car modes of transport and to mitigate the impact of the development on CHA signal junctions at the Staines Road West junctions of Tesco access road and Cadbury Road a contribution commensurate with the contribution from Costco would be necessary.”*

- 7.12 It is relevant to note that the applicant has submitted a Travel Plan which is designed to encourage the occupants to use more sustainable means of transport. £6,150 of the overall £60,150 financial contribution will be used to cover the cost of the CHA monitoring the Travel Plan. Subject to this, and the other financial contributions requested by the CHA, the impact on the highway network is considered acceptable.
- 7.13 In terms of parking provision, a total of 458 spaces are to be provided on the site. The development will comprise 26 no. customer parking spaces. A further 14 spaces will be available for staff and demonstration vehicles. All of these particular spaces will be situated outside the building. The remaining 418 parking spaces are to be provided within the building or at roof level. These are to be occupied by new and used cars for sale, valeting, workshop parking and other associated parking. Whilst Spelthorne Council's Parking Standards do not have a parking standard specifically for a car showroom/workshop of this nature, the CHA state that the developer is proposing adequate car parking in accordance with their own Parking Standards to accommodate visitor, staff, display, stock and repair vehicles, and that the parking area will not affect CHA roads. Highways England have also raised no objection on parking grounds. Accordingly, the proposed parking provision is considered acceptable.

#### Air Quality

- 7.14 Policy EN3 of the CS & P DPD states that the Council will seek to improve the air quality of the Borough and minimise harm from poor air quality by supporting appropriate measures to reduce traffic congestion where it is a contributor to existing areas of poor air quality. The policy also states that the Council will refuse development where the adverse effects on air quality are of a significant scale, either individually or in combination with other proposals, and which are not outweighed by other important considerations or effects and cannot be appropriately and effectively mitigated.
- 7.15 An Air Quality Assessment has been submitted with the application. It is recognised that the number of predicted traffic movements (and therefore likely vehicle emissions), when combined with those associated with the Costco development, will be greater compared to the former Dairy Crest development. However, this increase of vehicle movements is relatively small

compared to the overall movements (and emissions) on the adjacent A316 and surrounding area. The site is within a designated Employment Area, which is considered an appropriate location for this type of large scale commercial scheme in accordance with Policy EM1 of the CS & P DPD. It is not located immediately next to any existing residential properties, and the relatively small increase in vehicle movements and emissions is not likely to have a material impact on local residents. The Pollution Control Officer was consulted and has raised no objection subject to a financial contribution of £6,000 towards the cost of providing a public electric vehicle charging point (both fast and rapid) in the local area to help mitigate the impact of the likely increase in vehicle emissions caused by the development. The Officer has made the following comments:

*“Given the nature of the development and the constraints of the site, I cannot suggest any suitable on-site mitigation measures sufficient to offset the air quality impacts. It may therefore be necessary to consider financial compensation, under a Section 106 agreement, to enable the Council to put in place off-site mitigation. I recognise that some of the financial compensation measures requested by the County’s Highways engineer will have some emission reduction benefits through minimising the impacts of congestion from additional traffic movements through traffic management schemes for example. However, it is my opinion that some additional compensation would be appropriate. The scale of the compensation should be comparable to that of the Costco development, which I believe was a sum of £6,000. The Council is seeking to install public electric vehicle charging points (both fast and rapid) in the Sunbury Cross area and any S.106 monies would be put toward this project.”*

- 7.16 It is relevant to note that the CHA has requested a condition requiring 5% of the customer/staff parking spaces to have trickle electric charging points, which will encourage the use of electric vehicles and therefore help to reduce vehicle emissions. The Travel Plan will also encourage the use of more sustainable forms of transport.

#### Local Finance Considerations

- 7.17 Under S155 of the Housing and Planning Act 2016, Local Planning Authorities are now required to ensure that potential financial benefits of certain development proposals are made public when a Local Planning Authority is considering whether or not to grant planning permission for planning applications which are being determined by the Council’s Planning Committee. A financial benefit must be recorded regardless of whether it is material to the Local Planning Authority’s decision on a planning application, but planning officers are required to indicate their opinion as to whether the benefit is material to the application or not.
- 7.18 In consideration of S155 of the Housing and Planning Act 2016, the proposal will generate £66,150 towards highway improvements and sustainable transport measures. These are material considerations in the determination of this planning application. The proposal will also generate Business Rates payments which is not a material consideration in the determination of this proposal.

## Other Matters

- 7.19 The applicant is proposing to install air source heat pumps in order to achieve the 10% renewable energy requirement stipulated in Policy CC1 of the CS & P DPD. The Council's Sustainability Officer was consulted and is satisfied that the 10% requirement can be achieved using these particular facilities. Accordingly, the proposal complies with Policy CC1 of the CS & P DPD.
- 7.20 The site is located within Flood Zone 1, which has a low probability of flood risk with less than 1 in 1000 year chance of flooding. The applicant has submitted a Flood Risk Assessment and Drainage Strategy as is required by Policy LO1 of the CS & P DPD. The Environment Agency were consulted on the application. Whilst they have not responded, they did not wish to comment on the previous application on the site (15/00676/FUL) due to its location in a low risk flood zone. The Local Lead Flood Authority (Surrey County Council) were also consulted, particularly with regard to the Sustainable Drainage Scheme, and have raised no objection subject to conditions. The applicant is proposing to install a surface water underground attenuation unit, which is designed to absorb surface water run-off before discharging into the existing drainage network. The applicant is also proposing a permeable surface for the ground level parking spaces. The attenuation unit and permeable parking spaces will ensure that excessive surface water run-off from storm events is not discharged straight into the existing drainage network.
- 7.21 Accordingly, the application recommended for approval.

## **8. Recommendation**

- 8.1 (A) Subject to the applicant first entering into an appropriate legal agreement in respect of the following:
1. To provide the following financial contributions towards transport improvements and sustainable transport measures:
    - (a) £8,000 for passenger transport infrastructure improvements to mitigate the impact of the proposals, and to encourage less use of the private car and mode shift to public transport and therefore reduce emissions in the local area.
    - (b) £32,000 to revalidate the Urban Traffic Control (UTC) Regions in the vicinity of the development and to mitigate the impact of the proposals on the local junctions on the A308. £12,000 of this will go towards the UTC validation, however £20,000 of this is linked to a contingency which can be used if necessary.
    - (c) £14,000 for pedestrian and bicycle environment improvements towards improving pedestrian and bicycle facilities either along the A308 Staines Road West between the A308 junction with School Road and along the A316 to the proposed site access or at the A308 Staines Road West junction with School Road or along the A316 as far as the junction with the proposed site access.

(d) £6,150 one off cost for Surrey County Council to monitor the travel plan.

(e) £6,000 towards the cost of providing a public electric vehicle charging point in the Sunbury Cross area.

(B) GRANT subject to the following conditions: -

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason:- This condition is required by Section 91 of the Town and Country Planning Act, 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

2. The development hereby permitted shall be carried out in accordance with the following approved plans and drawings:

3230-020 Rev. C; /025 Rev. C; /030 Rev. V; /035 Rev. D; /040 Rev. R; /041 Rev. Q; /042 Rev. P; /043 Rev. J; /044 Rev. H; /050 Rev. M; /051 Rev. N; /060 Rev. F; /061 Rev. F; /062 Rev. F; /063 Rev. D; /064 Rev. E received 23 February 2017.

P16-199 250 Rev. A received 27 March 2017.

Reason:- For the avoidance of doubt and in the interest of proper planning

3. Before any work on the development hereby permitted is first commenced details of the materials and detailing to be used for the external surfaces of the building and the surface material for the parking spaces be submitted to and approved by the Local Planning Authority.

Reason:- To ensure that the proposed development does not prejudice the appearance of the development and the visual amenities and character of the locality, in accordance with policies SP6 and EN1 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

4. No development shall take place until:-

(a) A comprehensive desk-top study, carried out to identify and evaluate all potential sources and impacts of land and/or groundwater contamination relevant to the site, has been submitted to and approved in writing by the Local Planning Authority.

(b) Where any such potential sources and impacts have been identified, a site investigation has been carried out to fully characterise the nature and extent of any land and/or groundwater contamination and its implications. The site investigation shall not be commenced until the extent and methodology of the site investigation have been agreed in writing with the Local Planning Authority.



(c) A written method statement for the remediation of land and/or groundwater contamination affecting the site shall be agreed in writing with the Local Planning Authority prior to the commencement of remediation. The method statement shall include an implementation timetable and monitoring proposals, and a remediation verification methodology.

The site shall be remediated in accordance with the approved method statement, with no deviation from the statement without the express written agreement of the Local Planning Authority.

Reason:-

- (a) To protect the environment from the effects of potentially harmful substances.

#### NOTE

The requirements of the above Condition must be carried out in accordance with current best practice. The applicant is therefore advised to contact Spelthorne's Pollution Control team on 01784 446251 for further advice and information before any work commences. An information sheet entitled "Land Affected By Contamination: Guidance to Help Developers Meet Planning Requirements" providing guidance can also be downloaded from Spelthorne's website at [www.spelthorne.gov.uk](http://www.spelthorne.gov.uk).

In accordance with policies SP6 and EN15 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

5. No development shall take place until:-
- (i) Where any potential sources and impacts of land and groundwater contamination have been identified, a site investigation has been carried out to fully characterise the nature and extent of any land and/or groundwater contamination and its implications. The site investigation shall not be commenced until the extent and methodology of the site investigation have been agreed in writing with the Local Planning Authority.
- (ii) A written method statement for the remediation of land and/or groundwater contamination affecting the site shall be agreed in writing with the Local Planning Authority prior to the commencement of remediation. The method statement shall include an implementation timetable and monitoring proposals, and a remediation verification methodology.
- The site shall be remediated in accordance with the approved method statement, with no deviation from the statement without the express written agreement of the Local Planning Authority.

Reason:- To protect the environment from the effects of potentially harmful substances.

6. No development shall commence until a report has been submitted to and agreed by the Local Planning Authority which includes details and drawings demonstrating how 10% of the energy requirements

generated by the development as a whole will be achieved utilising renewable energy methods and showing in detail the estimated sizing of each of the contributing technologies to the overall percentage. The detailed report shall identify how renewable energy, passive energy and efficiency measures will be generated and utilised for each of the proposed buildings to meet collectively the requirement for the scheme. The agreed measures shall be implemented with the construction of each building and thereafter retained and maintained to the satisfaction of the Local Planning Authority unless otherwise agreed in writing.

Reason:- To ensure that the development is sustainable and complies with Policy SP7 and CC1 of the Spelthorne Development Plan Core Strategy and Policies DPD.

7. No development shall take place until there has been submitted to and approved in writing by the Local Planning Authority a plan indicating the positions, design, materials and type of boundary treatment to be erected. The boundary treatment shall be completed before the buildings are occupied. Development shall be carried out in accordance with the approved details and maintained as approved.

Reason:- To ensure that the proposed development does not prejudice the enjoyment by neighbouring occupiers of their properties and the appearance of the locality, in accordance with policies SP6 and EN1 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

8. No development shall take place until full details of both soft and hard landscape works have been submitted to and approved in writing by the Local Planning Authority and these works shall be carried out as approved. The trees, shrubs and other associated proposals shall be planted on the site within a period of 12 months from the date on which development hereby permitted is first commenced, or such longer period as may be approved by the Local Planning Authority, and that the planting so provided shall be maintained as approved for a period of 5 years, such maintenance to include the replacement in the current or next planting season whichever is the sooner, of any trees or shrubs that may die, are removed or become seriously damaged or diseased, with others of similar size and species, unless the Local Planning Authority gives written permission to any variation.

Reason:- To minimise the loss of visual amenity occasioned by the development and to enhance the proposed development.

9. Before the first occupation of any part of the development, a landscape management plan including long-term design objectives, management responsibilities and maintenance schedules for all landscaped areas shall be submitted to and approved in writing by the Local Planning Authority. The landscape management plan shall be carried out as approved.

Reason:- To minimise the loss of visual amenity occasioned by the development and to enhance the proposed development.

10. Demolition works and construction of the development hereby approved must only be carried out on site between 08:00 – 18:00 Monday to Friday, 08:00 – 13:00 Saturday and none at all on Sunday, Public Holidays or Bank Holidays.

Reason:- In the interest of amenity

11. Before any construction commences, details including a technical specification of all proposed external lighting shall be submitted to and approved in writing by the Local Planning Authority. The external lighting on the site shall at all times accord with the approved details.

Reason:- To safeguard the amenity of neighbouring residential properties, in the interest of security, and in the interest of wildlife.

12. No development shall commence until a Construction Transport Management Plan, to include details of:  
(a) Vehicle routing  
Has been submitted to and approved in writing by the Local Planning Authority. Only the approved details shall be implemented during the construction of the development.

Reason:- The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2012 and policy CC2 of Spelthorne Borough Council's Core Strategy and Policies Development Plan Document February 2009.

13. Notwithstanding the submitted plans the development hereby approved shall not be first occupied until 5% of the available customer/staff parking spaces have been provided with trickle electric vehicle charging points in accordance with a scheme to be submitted to and approved in writing by the Local Planning Authority. Thereafter the charging points shall be retained for that purpose.

Reason: The above condition is required in recognition of Section 4 (Promoting Sustainable Transport) of the NPPF.

14. Notwithstanding the submitted travel plan prior to the commencement of the development a Travel Plan shall be submitted for the written approval of the Local Planning Authority in accordance with the sustainable development aims and objectives of the National Planning Policy Framework, and Surrey County Council's "Travel Plans Good Practice Guide".

Reason: The above condition is required in recognition of Section 4 (Promoting Sustainable Transport) of the NPPF.

15. No new development shall be occupied until space has been laid out within the site in accordance with the approved plans for cars to be parked and for the loading and unloading of vehicles and for vehicles to turn so that they may enter and leave the site in forward gear. The parking/turning area shall be used and retained exclusively for its designated purpose.

Reason:- This condition is required in order that the development should not prejudice highway safety nor cause inconvenience to other highway users.

16. Prior to construction of the development hereby approved the following details shall be submitted to and approved by the local planning authority:
- Detailed development layout at an identified scale showing permeable and impermeable areas,
  - Detailing the exact location of SUDs elements, attenuation tank, Hydrobrake including finished floor levels and cover levels
  - Details of all SuDS elements, permeable paving and other drainage features (Hydrobrake) , including long and cross sections, pipe diameters and respective levels and how these relate to submitted calculations.

Reason: To ensure that the design fully meets the requirements of the national SuDS technical standards

17. Prior to construction of the development hereby approved, details of the proposed maintenance regimes for each of the SuDS elements must be submitted to and approved by the local planning authority.

Reason: To ensure the drainage system is maintained throughout its life time to an acceptable standard.

18. Before the commencement of the construction of the building hereby approved, details of how the Sustainable Drainage System will be protected and maintained during the construction of the development shall be submitted to and approved by the Local Planning Authority. The development shall thereafter be carried out in strict accordance with those approved details.

Reason: To ensure that the construction works do not compromise the functioning of the agreed Sustainable Drainage System.

19. Before the commencement of the construction of the development hereby approved, details of how the Sustainable Drainage System will cater for system failure or exceedance events, both on-site and off-site, must be submitted to and approved by the Local Planning Authority.

Reason: To ensure that the proposal has fully considered flood events exceeding design capacity and system failure.

20. Prior to occupation, a verification report carried out by a qualified drainage engineer must be submitted to and approved by the Local Planning Authority to demonstrate that the Sustainable Drainage System has been constructed as per the agreed scheme.

Reason: To ensure the Sustainable Drainage System is built to the approved drawings.

21. No goods or articles shall be stored on any part of the application site except inside the buildings.

Reason:- To ensure that the proposed development does not prejudice the appearance of the development and the visual amenities and character of the locality, in accordance with policies SP6 and EN1 of the Spelthorne Borough Core Strategy and Policies Development Plan Document 2009.

22. Prior to the occupation of the building hereby approved, a Noise Management Plan detailing delivery hours and other associated traffic movements and activities shall be submitted to and approved in writing by the Local Planning Authority. The requirements of the agreed Noise Management Plan shall be strictly complied with, unless otherwise agreed in writing by the Local Planning Authority.

Reason:- To safeguard the amenity of neighbouring residential properties.

23. All plant to be installed in association with the proposed development shall be at least 10 dB(A) below the background noise level at the nearest noise sensitive property as assessed using the guidance contained within BS 4142 (2015).

Reason: To safeguard the amenity of nearby residential properties.

24. Transport associated with the construction/erection of the development and other associated works shall not arrive on-site earlier than 07.30 hours and shall leave the site by 18.00 hours.

Reason:- To safeguard the amenity of nearby properties.

25. Prior to the commencement of construction, a scheme to provide bird and bat boxes on the site shall be submitted to and approved in writing by the Local Planning Authority. The agreed scheme shall be implemented before the buildings are occupied and thereafter maintained.

Reason:- To encourage wildlife on the site.

26. Development shall not commence until a drainage strategy detailing any on and/or off site drainage works, has been submitted to and approved in writing by the Local Planning Authority in consultation with the sewerage undertaker. No discharge of foul or surface water from

the site shall be acceptable into the public system until the drainage works referred to in the strategy have been completed.

Reason:- The development may lead to sewage flooding; to ensure that sufficient capacity is made available to cope with the new development; and in order to avoid adverse environmental impact upon the community.

27. No development shall take place until a Construction Environmental Management Plan (CEMP) for the site has been submitted and approved by the Local Planning Authority. The construction work shall be undertaken in strict accordance with this approved details, unless otherwise agreed in writing by the Local Planning Authority.

Reason:- To safeguard the amenity of neighbouring properties.

28. That the premises be not used for the purposes hereby permitted before 06:00 hours or after 23:00 hours on any day.

Reason:- To safeguard the amenity of neighbouring residential properties.

#### Informatives

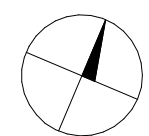
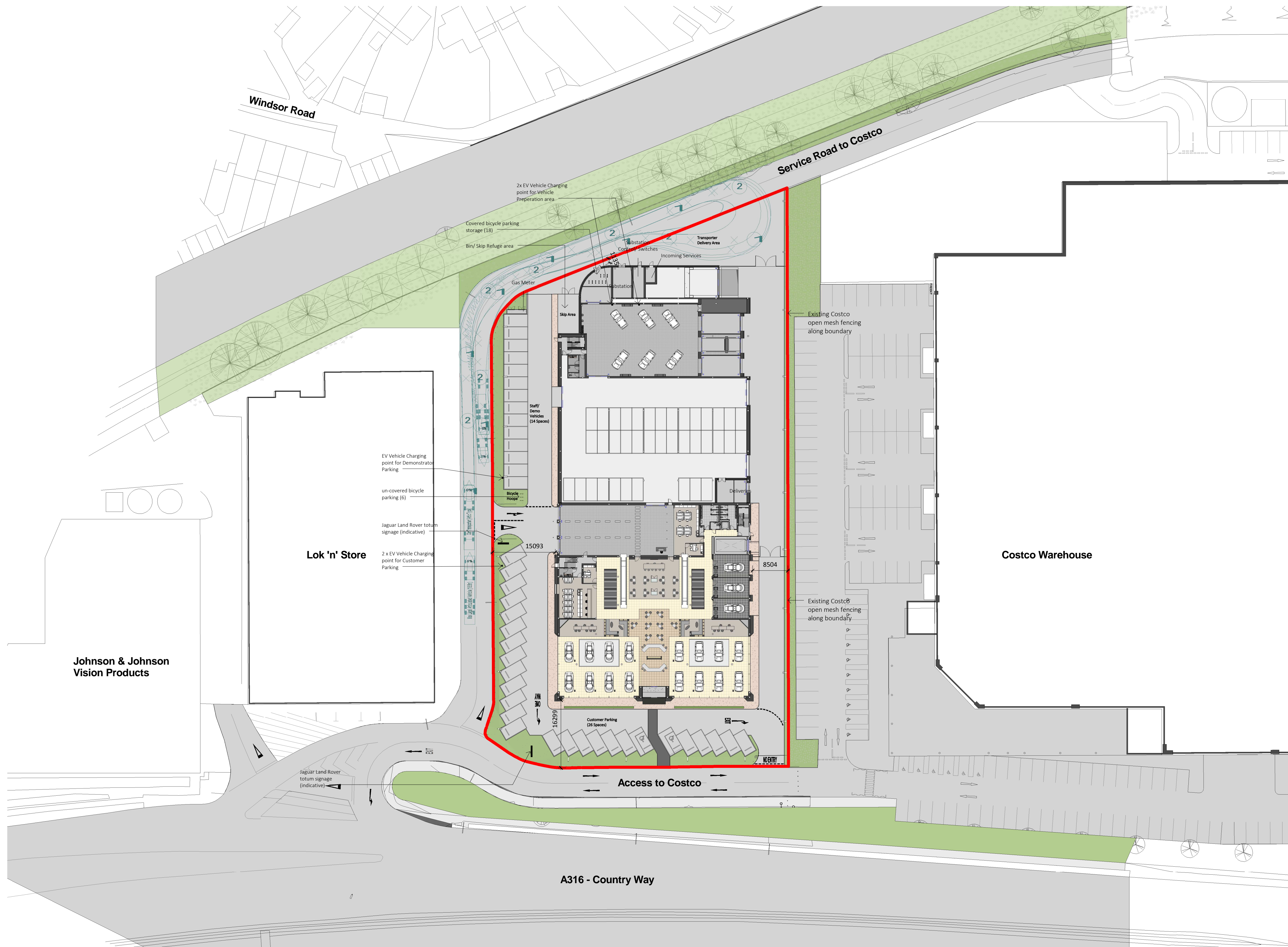
1. Design of attenuation tanks, permeable paving must take into account ground water levels during wet periods. High ground water tables can influence the attenuation tanks volume, and the inclusion of impermeable membrane (lining) will have to be considered.
2. No contaminated effluent from site shall be discharged to Ordinary Water Course. The Environmental Permitting Regulations make it an offence to cause or knowingly permit any discharge that will result in the input of pollutants to receiving water bodies.  
If you have any further queries regarding our response please contact the Sustainable Drainage and Consenting Team via email [SUDS@surreycc.gov.uk](mailto:SUDS@surreycc.gov.uk).
3. Thames Water will aim to provide customers with a minimum pressure of 10m head (approx. 1 bar) and a flow rate of 9 litres/minute at the point where it leaves Thames Water pipes.

#### **The Town and Country Planning (Development Management Procedure) (England) Order 2015** **Working in a positive/proactive manner**

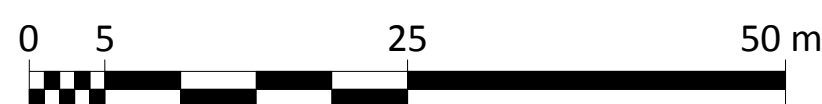
In assessing this application, officers have worked with the applicant in a positive and proactive manner consistent with the requirements of paragraphs 186-187 of the NPPF. This included the following:-

- a) Provided pre-application advice to seek to resolve problems before the application was submitted and to foster the delivery of sustainable development.
- b) Provided feedback through the validation process including information on the website, to correct identified problems to ensure that the application was correct and could be registered;
- c) Have suggested/accepted/negotiated amendments to the scheme to resolve identified problems with the proposal and to seek to foster sustainable development.
- d) Have proactively communicated with the applicant through the process to advise progress, timescales or recommendation.





**1 Site Plan**  
1 : 500



**Key**

Red line denotes site boundary

Note:  
Signage to form part of separate application

**JLR Sunbury**

Drawing title  
**Site Plan - Proposed**

Drawing number & revision  
**3230/030 V**

Amendments  
**Updated Landscape plan for Planning**

Drawn by  
**PL**

Checked by  
**MR/EG**

A1 Scale  
**1 : 500**

A3 Scale

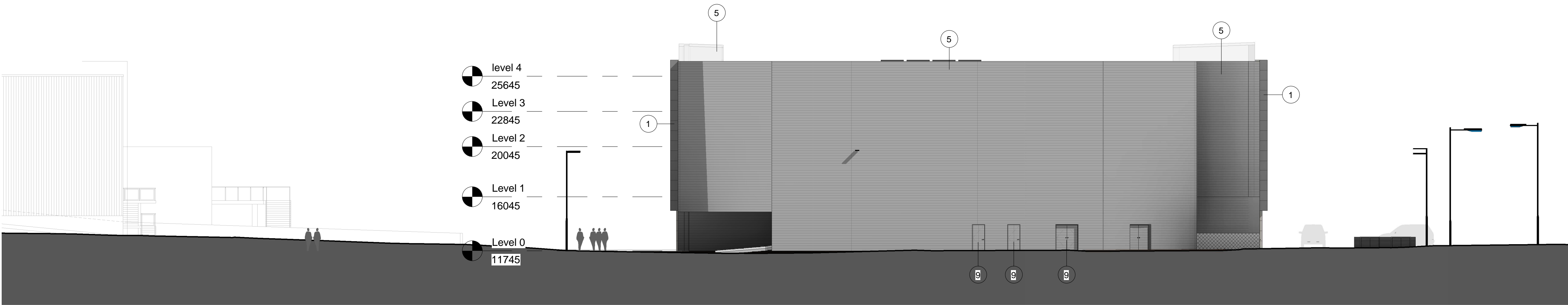
Date  
**15/02/2017  
11:28:01**

Status  
**Planning**

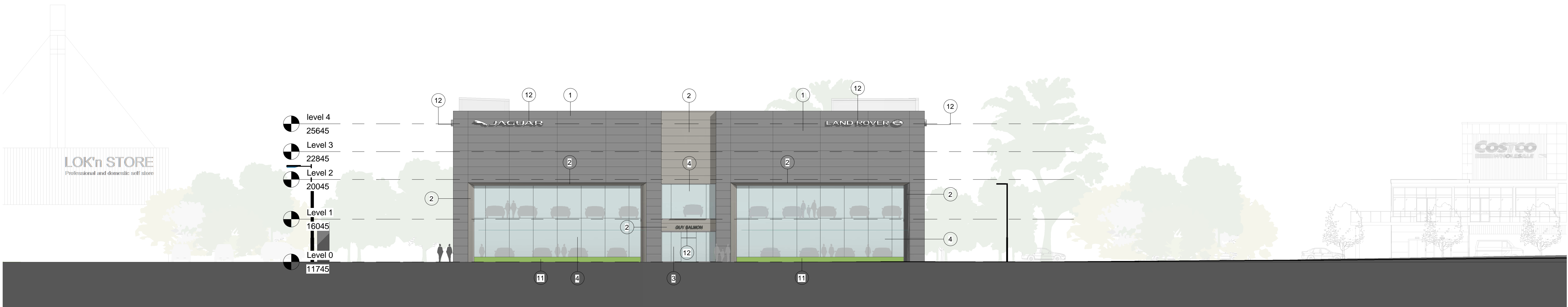
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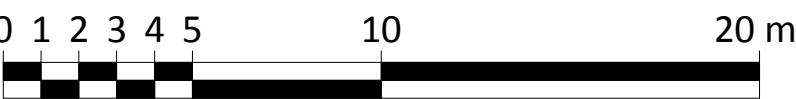




1 North elevation  
1 : 200



2 South elevation  
1 : 200



Material Legend

- 1 JLR Dark rainscreen cladding - 'Sunshine Grey'
- 2 JLR Light rainscreen cladding - 'Champagne Silver'
- 3 Glazed entry doors
- 4 Frameless structural glazing
- 5 Trapazoidal horizontal cladding
- 6 Glazed sectional door
- 7 Light coloured roller shutter vehicle door
- 8 Dark personnel door
- 9 Light personnel door
- 10 Punched window
- 11 Box hedging
- 12 Signage

JLR Sunbury

Drawing title		Drawing number & revision		Amendments	
Elevation North & South - Proposed		3230/ 051 N		Planning Issue	
Drawn by	Checked by	A1 Scale	A3 Scale	Date	Status
PL	MR/EG	1 : 200		02/02/2017 12:15:14	Planning

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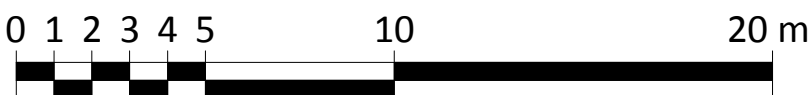
1 East elevation  
1 : 200



2 West elevation  
1 : 200

**Material Legend**

- 1 JLR Dark rainscreen cladding - 'Sunshine Grey'
- 2 JLR Light rainscreen cladding - 'Champagne Silver'
- 3 Glazed entry doors
- 4 Frameless structural glazing
- 5 Trapezoidal horizontal cladding
- 6 Glazed sectional door
- 7 Light coloured roller shutter vehicle door
- 8 Dark personnel door
- 9 Light personnel door
- 10 Punched window
- 11 Box hedging
- 12 Signage



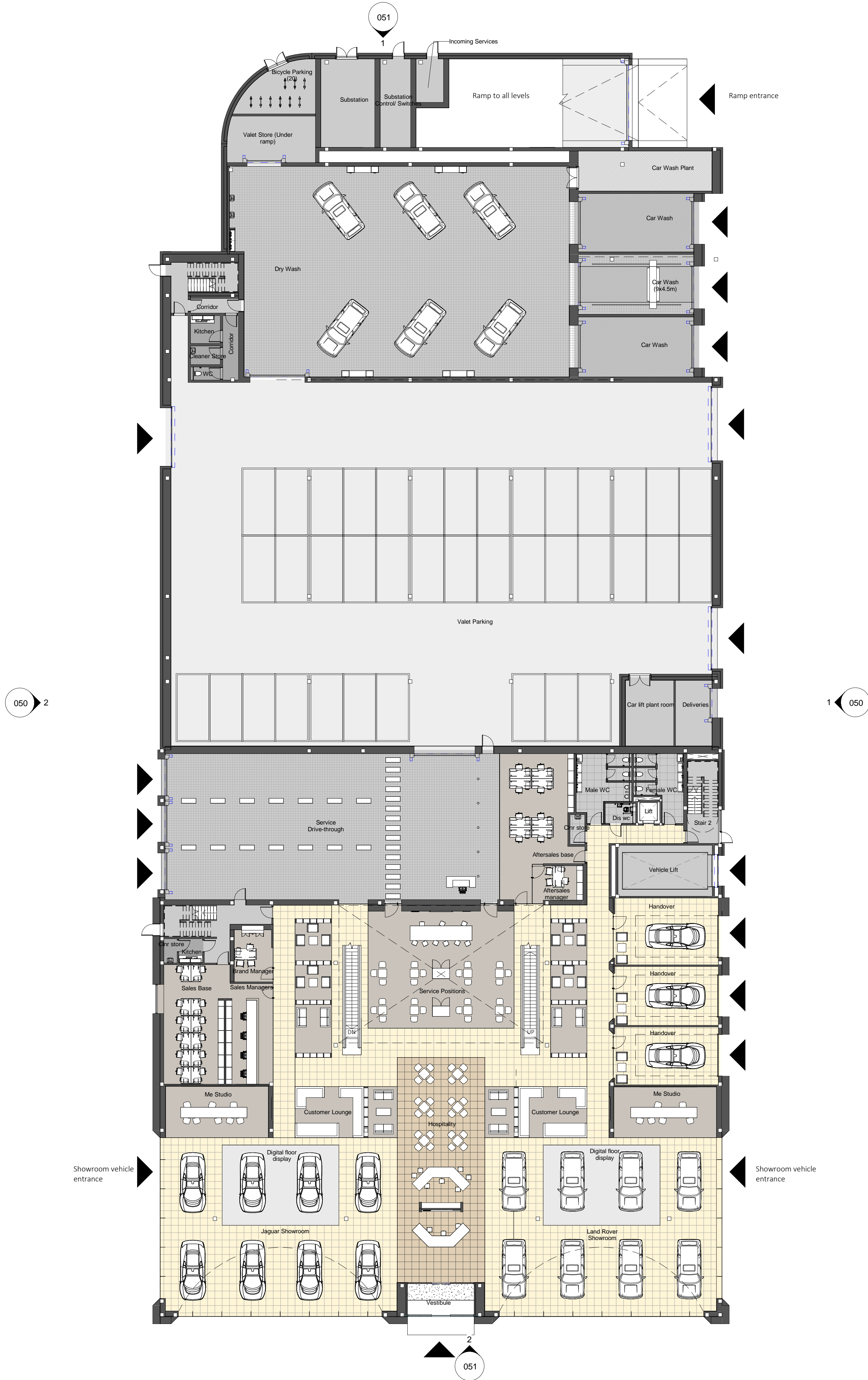
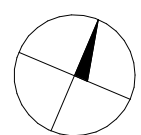
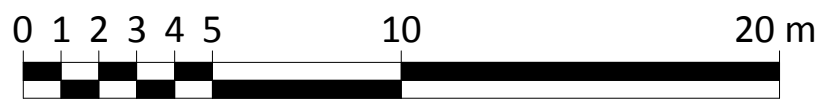
**JLR Sunbury**

Drawing title		Drawing number & revision		Amendments	
Elevations East & West - Proposed		3230/ 050 M		Planning Issue	
Drawn by	Checked by	A1 Scale	A3 Scale	Date	Status
PL	MR/EG	1 : 200		02/02/2017 12:14:06	Planning

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1 Ground Floor Plan  
1 : 200

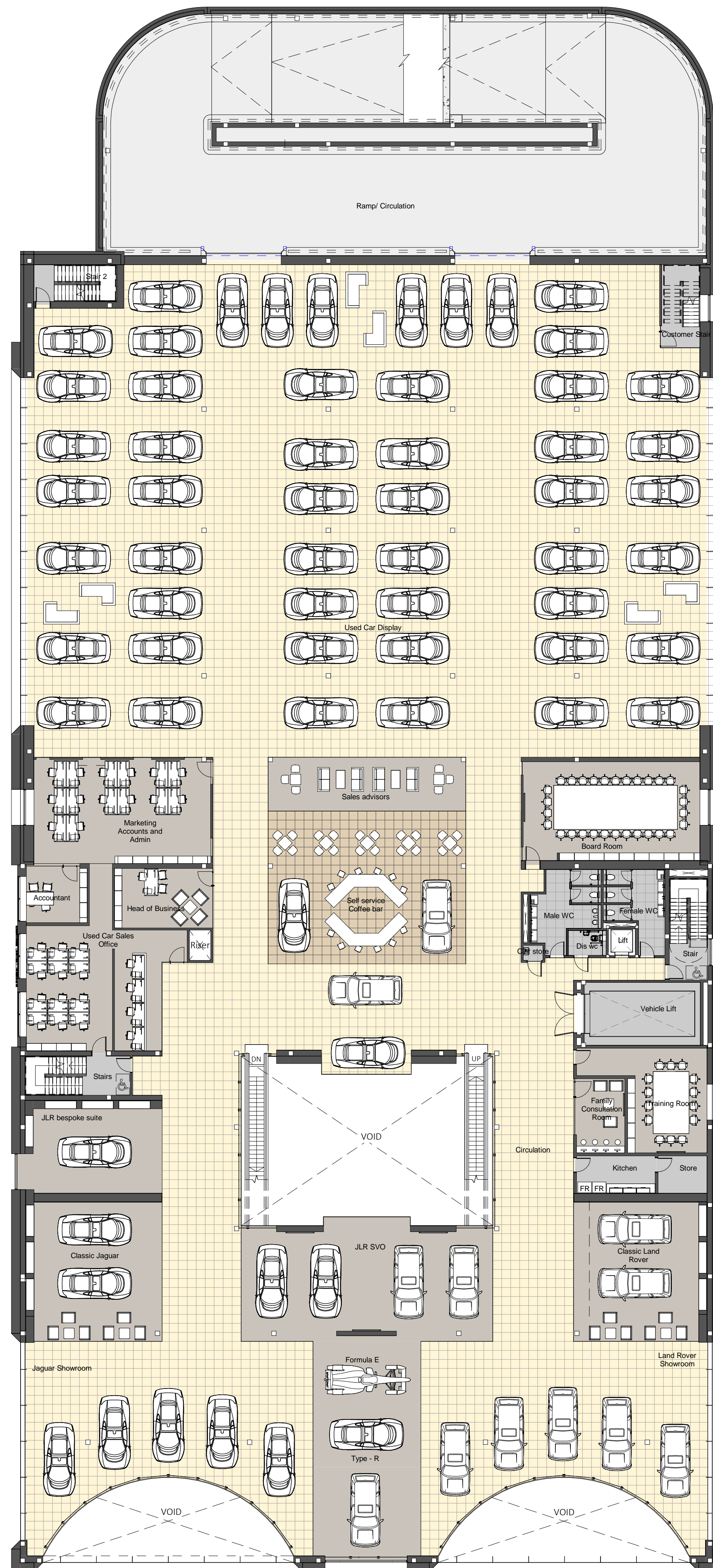


JLR Sunbury

Drawing title	Drawing number & revision	Amendments			
Ground Floor Plan - Proposed	3230/ 040 R	Planning Issue			
Drawn by	Checked by	A1 Scale	A3 Scale	Date	Status
PL	MR/EG	As indicated		02/02/2017 12:11:56	Planning

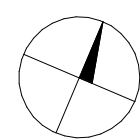
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1 Level 1  
1 : 200

0 1 2 3 4 5 10 20 m

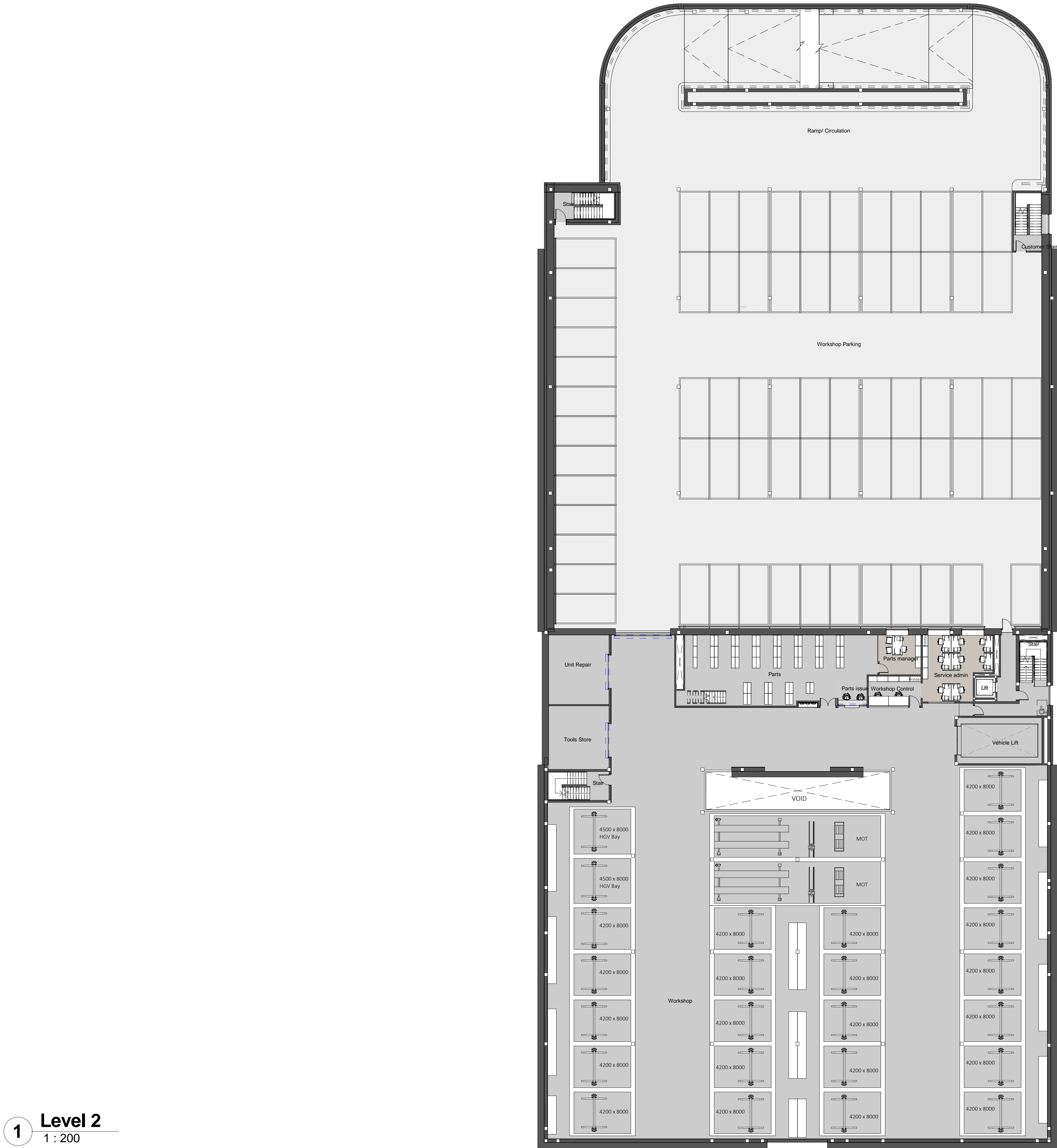


## JLR Sunbury

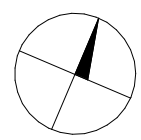
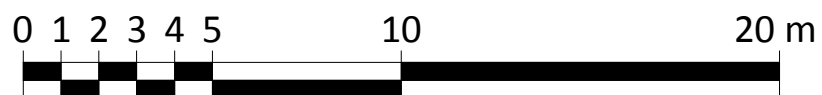
Drawing title		Drawing number & revision		Amendments	
First Floor Plan - Proposed		3230/ 041 Q		Planning Issue	
Drawn by	Checked by	A1 Scale	A3 Scale	Date	Status
PL	MR/EG	As indicated		26/01/2017 10:24:28	Planning

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**1 Level 2**  
1 : 200

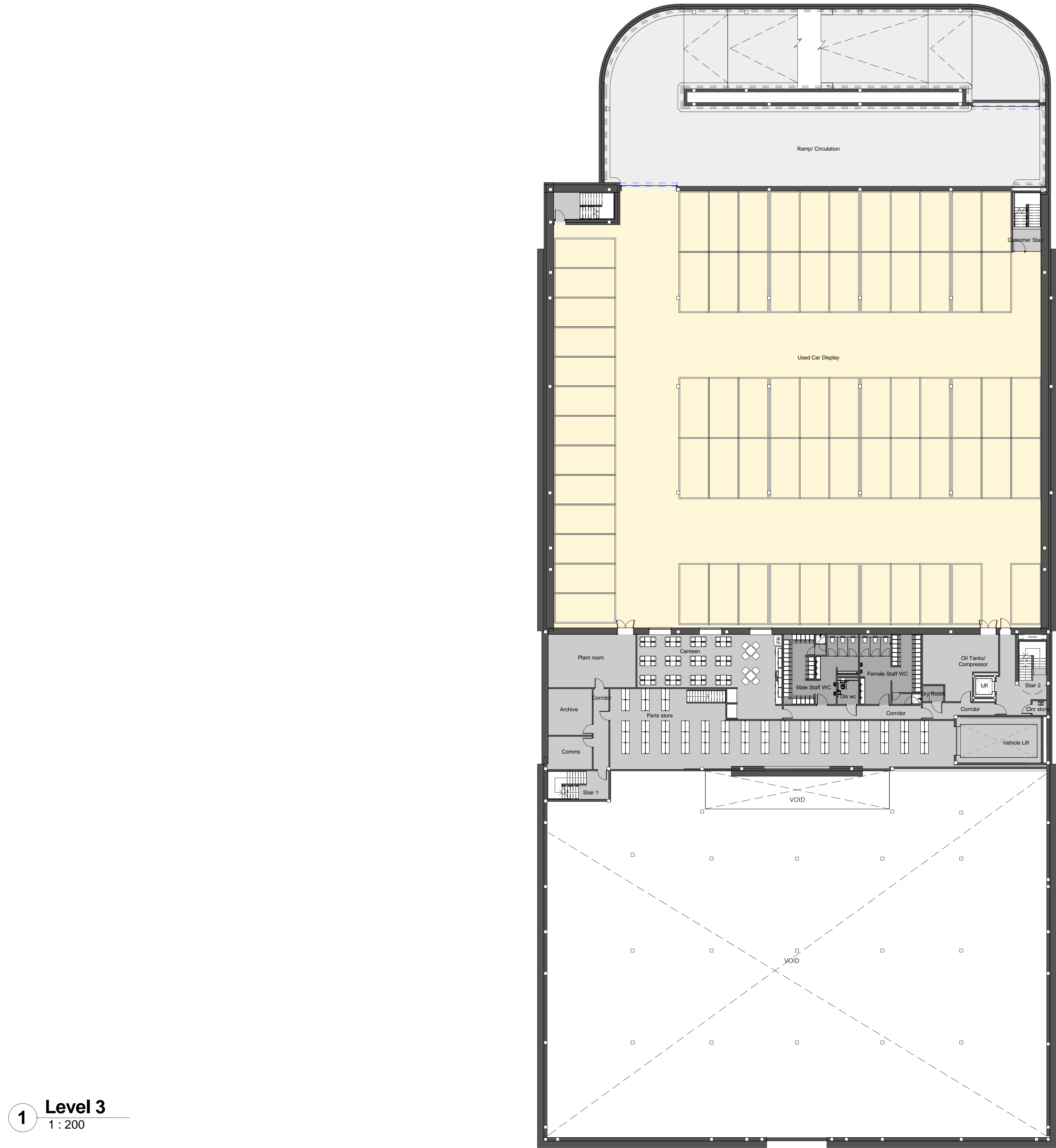


## JLR Sunbury

Drawing title		Drawing number & revision		Amendments	
Second Floor Plan - Proposed		3230/ 042 P		Planning Issue	
Drawn by	Checked by	A1 Scale	A3 Scale	Date	Status
PL	MR/EG	As indicated		26/01/2017 10:11:28	Planning

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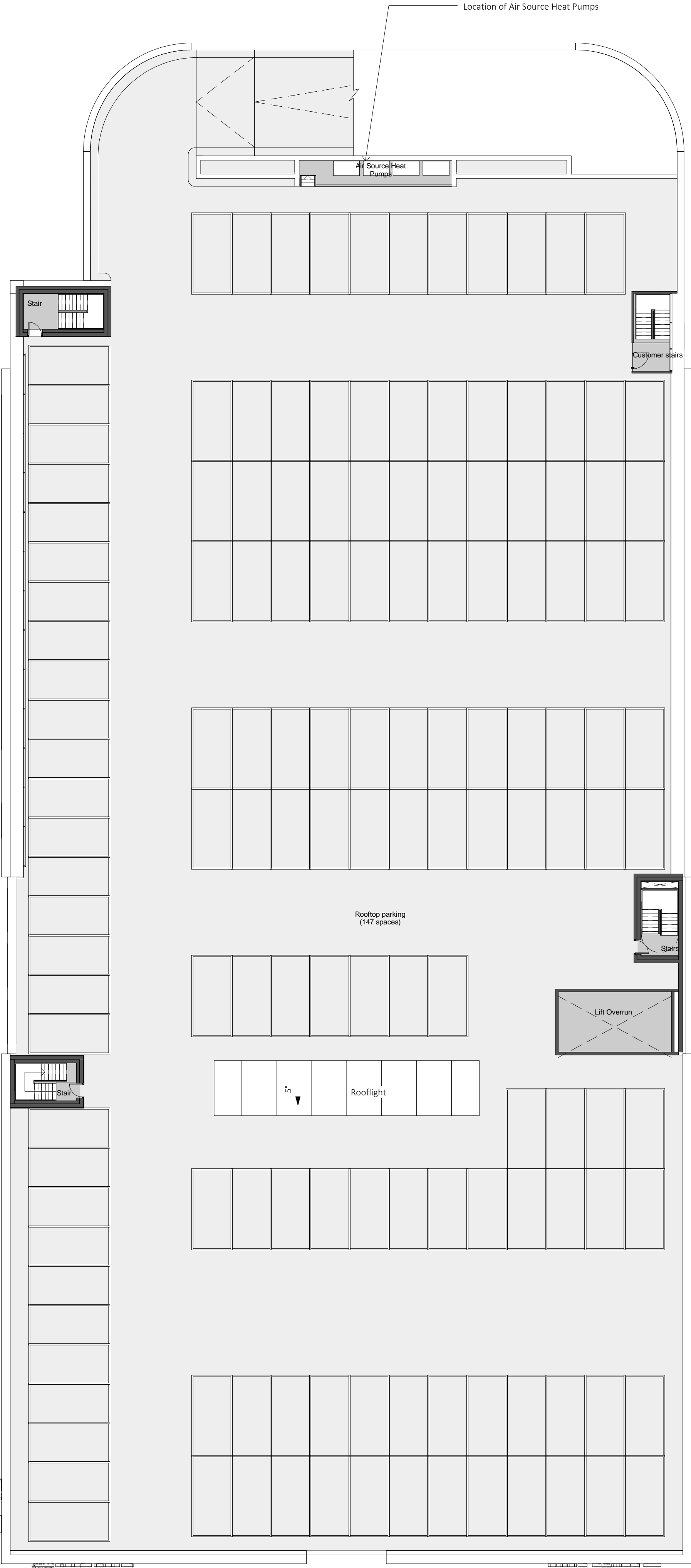


**1 Level 3**  
1 : 200

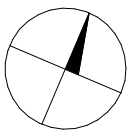
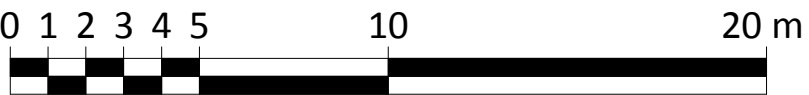
JLR Sunbury

Drawing title		Drawing number & revision		Amendments	
Third Floor Plan - Proposed		3230/ 043 J		Planning Issue	
Drawn by	Checked by	A1 Scale	A3 Scale	Date	Status
PL	MR/EG	As indicated		02/02/2017 12:11:59	Planning

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**1 Level 4 Roof**  
1 : 200



## JLR Sunbury

Drawing title		Drawing number & revision		Amendments	
Fourth Floor Roof Plan		3230/ 044 H		Planning Issue	
- Proposed					
Drawn by	Checked by	A1 Scale	A3 Scale	Date	Status
PL	MR/EG	As indicated		26/01/2017 10:11:37	Planning

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